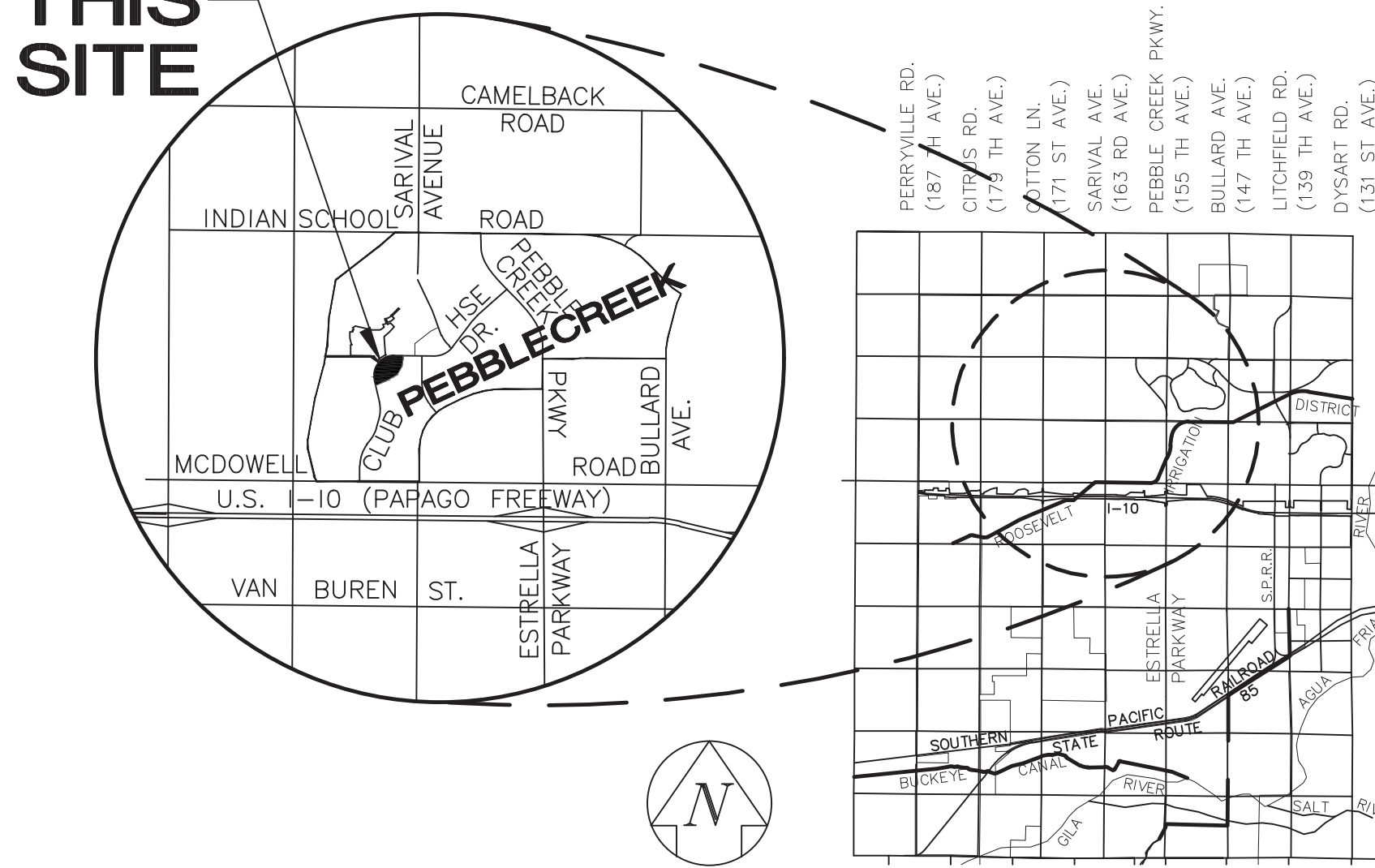
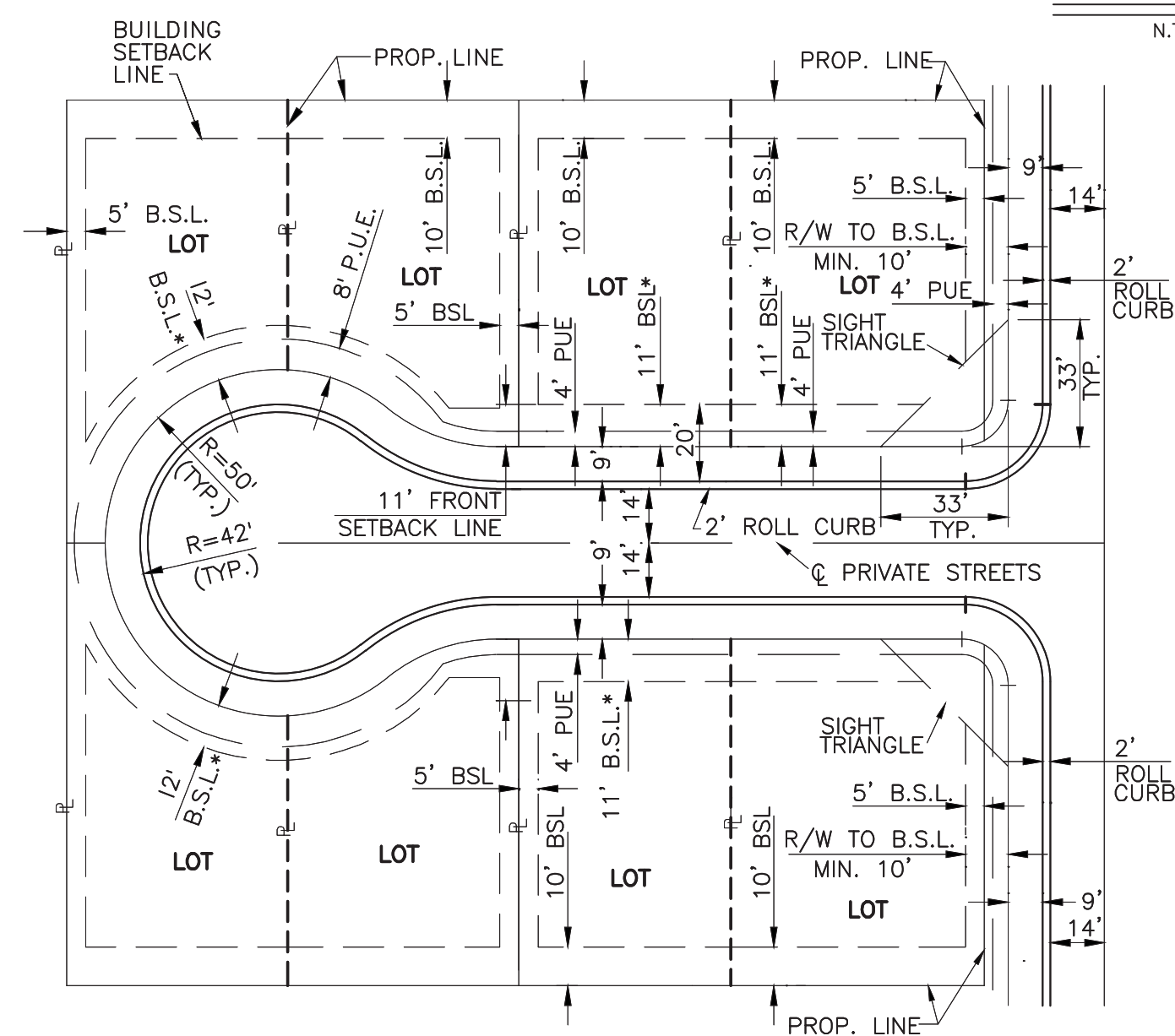


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VICINITY MAP

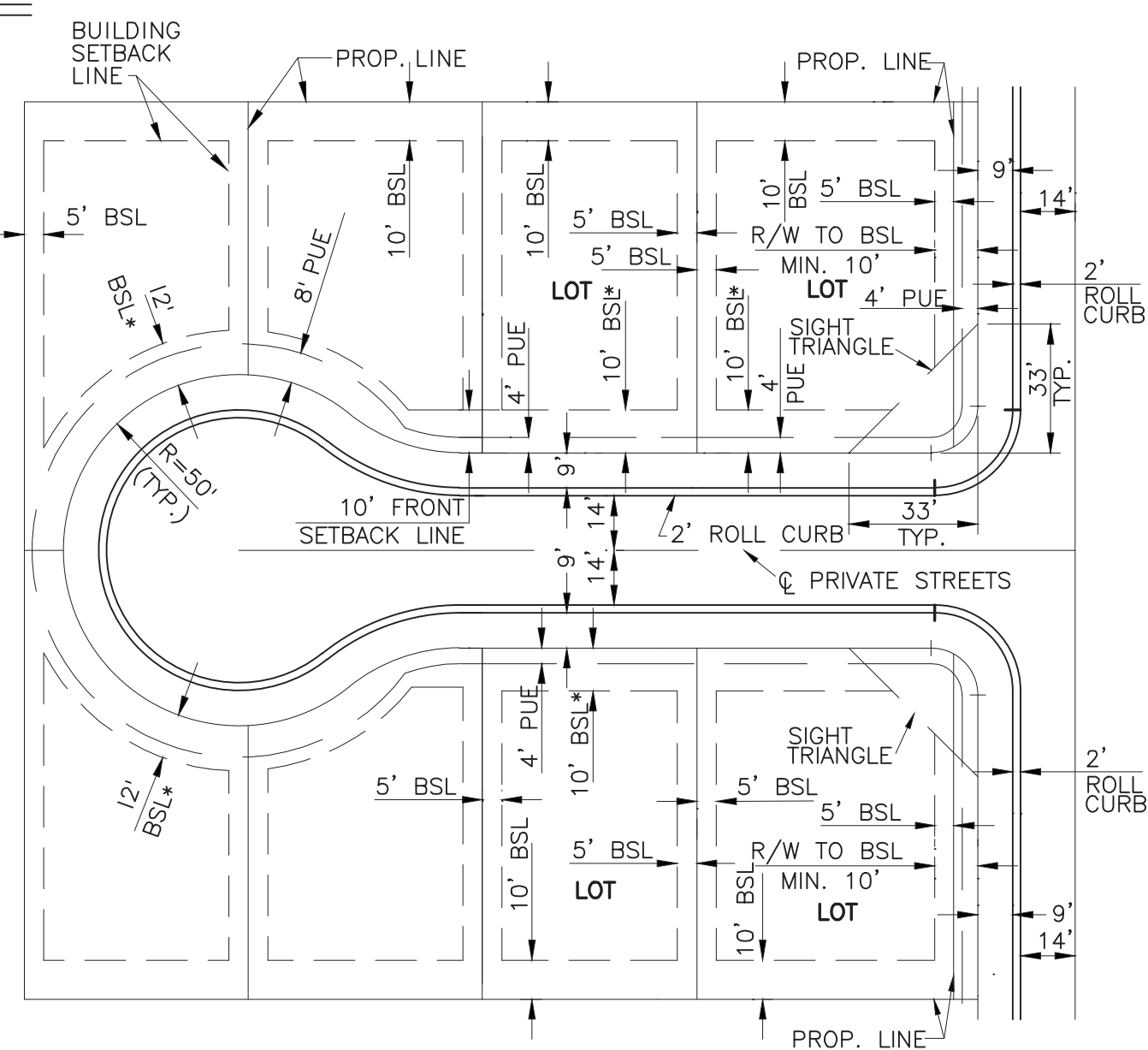


TYPICAL LOTS
"VILLA SERIES HOMES"
LOTS 1-36

WITH PUBLIC UTILITY EASEMENTS AND
MINIMUM BUILDING SETBACKS

P.U.E. - INDICATES PUBLIC UTILITY EASEMENT
B.S.L. - INDICATES BUILDING SETBACK LINE
--- ZERO SETBACK LINE FOR DUPLEX LOTS

TYPICAL VILLA LOTS SHOWING SETBACK LINES
AND PUBLIC UTILITY EASEMENTS
(UNLESS OTHERWISE SHOWN)
MIN. FRONT YARD SETBACK = 11 FT.
MIN. STREET SIDE SETBACK = 10 FT. (MIN. TO R/W)
MIN. REAR YARD SETBACK = 10 FT.
MIN. SIDE YARD SETBACK = 5 FT.
* FRONT YARD BUILDING SETBACKS TO BE INCREASED
BY 5' ADJACENT TO SIDEWALKS
BUILDING SETBACK LINE TO BE 12 FEET WITHIN
THE CUL-DE-SAC TO ALLOW FOR A 20 FOOT DRIVEWAY

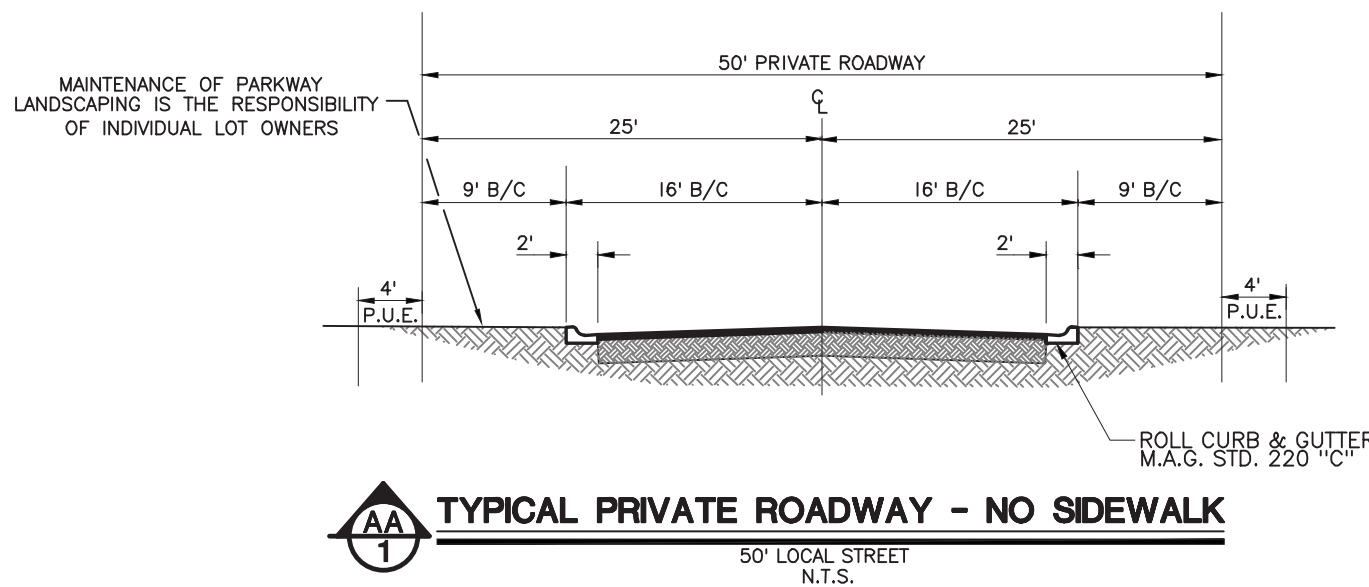


TYPICAL LOTS
LOTS 38-110

WITH PUBLIC UTILITY EASEMENTS AND
MINIMUM BUILDING SETBACKS

P.U.E. - INDICATES PUBLIC UTILITY EASEMENT
B.S.L. - INDICATES BUILDING SETBACK LINE

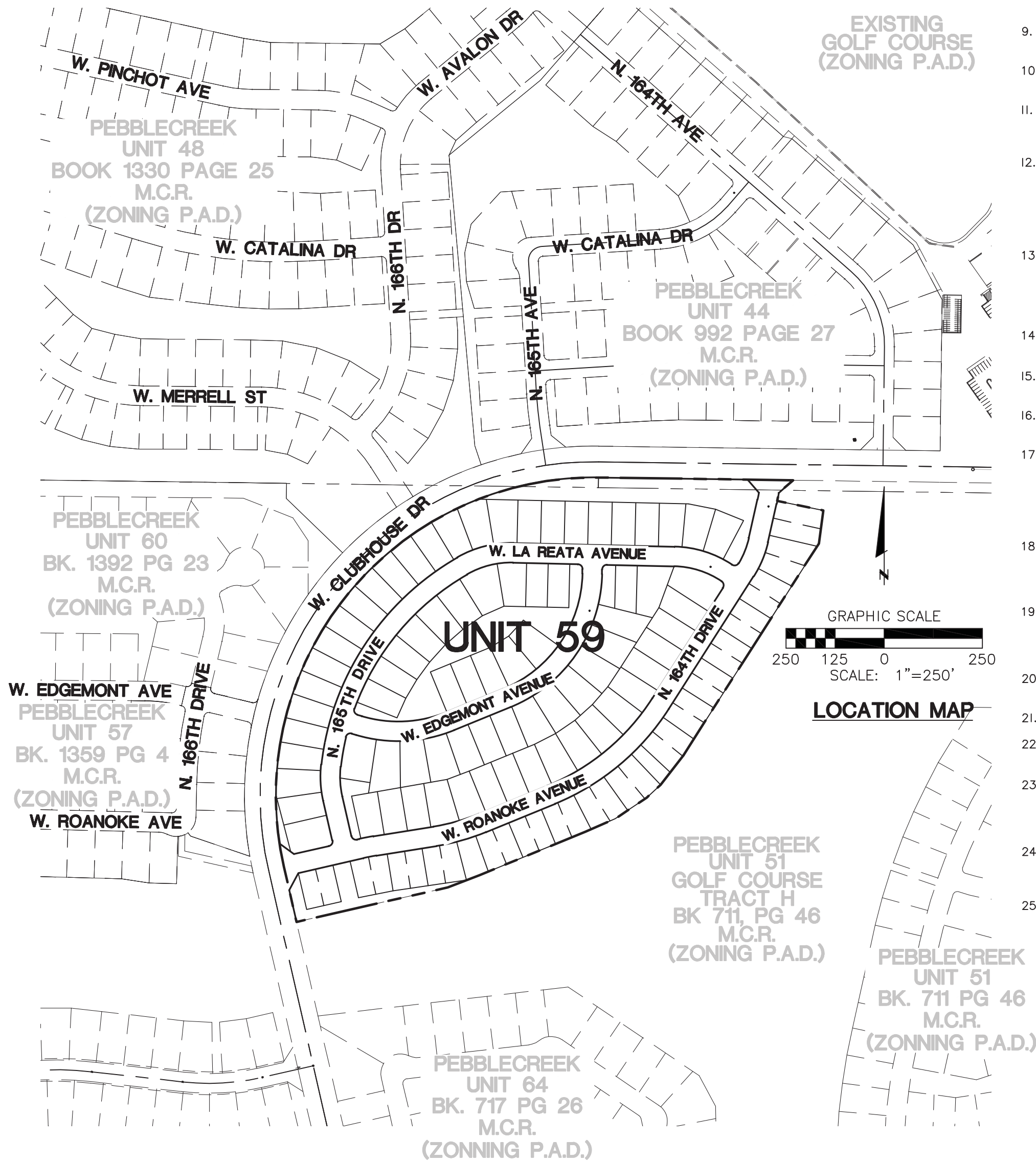
TYPICAL LOTS SHOWING SETBACK LINES
AND PUBLIC UTILITY EASEMENTS
(UNLESS OTHERWISE SHOWN)
SCALE: 1" = 40'
MIN. FRONT YARD SETBACK = 10 FT.
MIN. STREET SIDE SETBACK = 10 FT.
MIN. REAR YARD SETBACK = 10 FT.
MIN. SIDE YARD SETBACK = 5 FT.
* FRONT YARD BUILDING SETBACKS TO BE INCREASED
BY 5' ADJACENT TO SIDEWALKS
BUILDING SETBACK LINE TO BE 12 FEET WITHIN
THE CUL-DE-SAC TO ALLOW FOR A 20 FOOT DRIVEWAY



TYPICAL PRIVATE ROADWAY - NO SIDEWALK
50' LOCAL STREET
N.T.S.

PRELIMINARY PLAT OF PEBBLECREEK PHASE II UNIT FIFTY-NINE CONSISTING OF 2 SHEETS

LOCATED IN PORTION OF THE NORTHEAST QUARTER OF SECTION 36 AND A
PORTION OF THE SOUTHEAST QUARTER OF SECTION 25, T2N, R2W GILA AND
SALT RIVER MERIDIAN, CITY OF GOODYEAR, MARICOPA COUNTY ARIZONA



GRAPHIC SCALE
250 125 0
SCALE: 1"=250'
LOCATION MAP

GENERAL NOTES

- EXISTING ZONING IS P.A.D.
- PEBBLECREEK PHASE II IS SUBJECT TO THE 1998 DEVELOPMENT AGREEMENT AND THE 2005 DEVELOPMENT AGREEMENT FOR PEBBLECREEK RESORT COMMUNITY.
- PEBBLECREEK PROPERTY LIMITED PARTNERSHIP WILL ACCEPT RESPONSIBILITY FOR MAINTENANCE OF ALL COMMONLY-OWNED ELEMENTS, LANDSCAPING OF OPEN AREAS AND PRIVATE STREETS WITHIN TRACTS UNTIL SUCH TIME TRACTS ARE FORMALLY CONVEYED TO THE HOMEOWNERS ASSOCIATION(S).
- ONCE TRACTS ARE CONVEYED TO THE HOMEOWNERS ASSOCIATION(S) THE ASSOCIATION(S) WILL BE RESPONSIBLE FOR ALL MAINTENANCE OF THESE TRACTS.
- ALL PROPOSED DWELLINGS SHALL BE SINGLE FAMILY.
- PRIVATE STREETS WILL BE RESERVED FOR THE USE, BENEFIT AND CONVENIENCE OF ALL OWNERS OF PROPERTY WITHIN THIS SUBDIVISION AND SUBSEQUENT PEBBLECREEK UNITS, THEIR GUESTS AND INVITEES, AND FOR INGRESS AND EGRESS OF FIRE, EMERGENCY, GOVERNMENTAL AND REFUSE COLLECTION VEHICLES.
- NON-EXCLUSIVE EASEMENTS, FOR UTILITY PURPOSES, WILL BE CREATED WITHIN THE ONSITE PRIVATE ROADWAYS, DENOTED AS TRACT "A" ON THIS PLAT. DRAINAGE AND UNDERGROUND UTILITY EASEMENTS WILL ALSO BE GRANTED TO UTILITY COMPANIES CONSISTING OF FOUR (4) FEET ALONG ALL PRIVATE STREETS.
- ALL STREETS SHOWN HEREIN ARE PRIVATE, AND ALL STREET SIGNS AND STREET LIGHTS WILL BE OWNED AND MAINTAINED BY THE HOMEOWNERS ASSOCIATION.
- DRIVEWAYS ON CORNER LOTS ARE TO BE LOCATED ON THE SIDE OF THE LOT FURTHEST FROM THE STREET INTERSECTION.
- DRIVEWAYS ON KEY LOTS ARE TO BE LOCATED ON THE SIDE OF THE LOT OPPOSITE THE ADJACENT REAR LOT LINE.
- WHERE A CORNER LOT ABUTS A KEY LOT, THE HEIGHT AND LOCATION OF WALLS AND FENCES IN THE YARD FACING THE SIDE STREET MUST CONFORM TO THE FRONT YARD REQUIREMENTS FOR WALLS AND FENCES.
- PEBBLECREEK GOLF RESORT PHASE II IS SUBJECT TO ATTENDANT NOISE, VIBRATION, DUST, AND ALL OTHER EFFECTS THAT MAY BE CAUSED BY OVERFLIGHTS AND BY THE OPERATION OF AIRCRAFT LANDING AT, OR TAKING OFF FROM LUKE AIR FORCE BASE; AND THE OWNER DOES FURTHER RELEASE AND DISCHARGE THE CITY, PEBBLECREEK PROPERTIES LIMITED PARTNERSHIP AND ROBSON COMMUNITIES AND ITS AFFILIATES, FROM ANY LIABILITY FOR ANY AND ALL CLAIMS FOR FUTURE DAMAGES AND COMPLAINTS OF ANY KIND TOPERSONS OR PROPERTY THAT MAY ARISE AT ANY TIME IN THE FUTURE.
- THE DEVELOPER/BUILDER SHALL HAVE A DISCLOSURE NOTICE, WHICH SHALL INCLUDE A MAP CALLING OUT THE SOUND CONTOUR LINES IN A FONT THAT IS NOT LESS THAN SIZE 16, PLACED IN ANY SALES OFFICE IN A PROMINENT LOCATION INFORMING POTENTIAL BUYERS OF THE DISTANCE TO ACCIDENT POTENTIAL ZONE II AND NOISE CONTOUR LINES FOR LUKE AIR FORCE BASE.
- THIS DEVELOPMENT IS IN CLOSE PROXIMITY TO THE ARIZONA MOTORSPORT PARK (GENERALLY LOCATED AT CAMELBACK ROAD & REEMS ROAD) AND IS SUBJECT TO NOISE INTRUSION.
- THIS DEVELOPMENT IS SUBJECT TO ATTENDANT NOISE AND VISIBLE INTRUSION CAUSED FROM VEHICULAR TRAFFIC ON INTERSTATE 10 AND THE LOOP 303 EXTENSION.
- THIS DEVELOPMENT IS ADJACENT TO AGRICULTURAL USES AND IS THEREFORE SUBJECT TO NOISE, DUST, AND ODDORS ASSOCIATED WITH SUCH A USE.
- NO STRUCTURES OF ANY KIND SHALL BE CONSTRUCTED OR PLACED WITHIN OR OVER THE UTILITY EASEMENTS EXCEPT: UTILITIES, ASPHALT OR CONCRETE PAVING, GRASS, AND WOOD, WIRE OR REMOVABLE SECTION TYPE FENCING. IT SHALL BE FURTHER UNDERSTOOD THAT THE CITY OF GOODYEAR SHALL NOT BE REQUIRED TO REPLACE ANY OBSTRUCTIONS, PAVING OR PLANTING THAT MUST BE REMOVED DURING THE COURSE OF MAINTENANCE, CONSTRUCTION OR RECONSTRUCTION.
- NO STRUCTURE OF ANY KIND SHALL BE CONSTRUCTED OR ANY VEGETATION BE PLANTED NOR BE ALLOWED TO GROW WITHIN, ON OR OVER ANY DRAINAGE EASEMENT WHICH WOULD OBSTRUCT OR DIVERT THE FLOW OF STORM WATER. THE CITY MAY, IF IT SO DESIRES, CONSTRUCT OR MAINTAIN DRAINAGE FACILITIES ON OR UNDER THE LAND OF THE DRAINAGE EASEMENT.
- STRUCTURES AND LANDSCAPING WITHIN VISIBILITY EASEMENTS WILL BE LIMITED TO GROUND COVER, FLOWERS AND GRANITE LESS THAN 2 FEET (MATURE) IN HEIGHT AND/OR TREES WITH BRANCHES NOT LESS THAN 6 FEET ABOVE THE GROUND. TREES SHALL BE PLACED AT LEAST 8 FEET APART.
- ALL SHADE STRUCTURES IN THE REAR YARD SHALL BE APPROVED BY THE PEBBLECREEK ARCHITECTURAL CONTROL COMMITTEE.
- WHEELCHAIR RAMPS WILL BE PROVIDED ON SIDEWALKS PER A.D.A. REQUIREMENTS.
- ALL NEW OR RELOCATED PUBLIC UTILITIES WILL BE PLACED UNDERGROUND, WITH THE EXCEPTION OF ANY 69KV ELECTRICAL LINES.
- THE COMPLETION OF ROAD IMPROVEMENTS (PAVING, CURB, GUTTER, SIDEWALK, STREET LIGHTING, AND LANDSCAPING) WITHIN AND ABUTTING THE SUBDIVISION TO BE PER THE APPLICABLE ENGINEERING DESIGN STANDARDS OR IN ACCORDANCE WITH APPROVED EXCEPTIONS AS ESTABLISHED BY THE PEBBLE CREEK DEVELOPMENT AGREEMENT AND PLANNED AREA DEVELOPMENT.
- MAINTENANCE OF ALL STREET LIGHTING AND LANDSCAPING SHALL BE THE RESPONSIBILITY OF PEBBLECREEK HOMEOWNERS ASSOCIATION UNLESS OTHERWISE NOTED ON THIS PLAT. THIS INCLUDES LANDSCAPING WITHIN THE RIGHT-OF-WAY, PRIVATE STREET TRACT, AND/OR PUE.
- CORNER LOTS ARE RESTRICTED TO ONE STORY.

PUBLIC UTILITIES

WATER LIBERTY UTILITIES
SEWER LIBERTY UTILITIES
ELECTRIC ARIZONA PUBLIC SERVICE
GAS SOUTHWEST GAS COMPANY
TELEPHONE CENTURYLINK
SOLID WASTE DISPOSAL CITY OF GOODYEAR
POLICE PROTECTION CITY OF GOODYEAR
(AND A PRIVATE PATROL)
FIRE PROTECTION AND
EMERGENCY SERVICES DISPATCH CITY OF GOODYEAR
CABLE TELEVISION COX CABLE

NOTE: (IMPROVEMENTS)

- ALL PUBLIC UTILITIES TO BE UNDERGROUND, WITH THE EXCEPTION OF EXISTING 69 KV POWER LINES. ALL STREETS TO BE IMPROVED WITH ASPHALT PAVEMENT AND CONCRETE CURB AND GUTTER. ALL IMPROVEMENTS TO COMPLY WITH M.A.G. STANDARDS AND SPECIFICATIONS AND/OR CITY OF GOODYEAR STANDARD MODIFICATIONS.
- DEVELOPER OR SUCCESSORS SHALL WARRANTY ALL PUBLIC IMPROVEMENTS CONSTRUCTED WITHIN CITY OF GOODYEAR RIGHTS-OF-WAY AND EASEMENTS FOR A PERIOD OF NOT LESS THAN TWO (2) YEARS FROM THE DATE OF ACCEPTANCE BY THE CITY.

BENCHMARKS

C.O.G. BENCHMARK G357 - FOUND BRASS CAP IN HAND HOLE AT INTERSECTION OF THOMAS ROAD AND PEBBLECREEK PARKWAY (S.W. CORNER SECTION 29) ELEVATION = 1014.50 NGVD29 (PROJECT DATUM).
=1016.38 NAVD88
C.O.G. BENCHMARK G371 - FOUND BRASS CAP IN HAND HOLE AT INTERSECTION OF CAMELBACK ROAD AND ALSUP (1/2 MILE WEST OF REEMS ROAD)
ELEVATION = 1054.59 NGVD29 (PROJECT DATUM).
=1056.34 NAVD88

ENGINEER

B&R ENGINEERING, INC.
9666 E. RIGGS ROAD SUITE 118
SUN LAKES, ARIZONA 85248
PHONE (480) 895-0799

OWNER/TRUSTEE

FIRST AMERICAN TITLE INSURANCE CO.
UNDER TRUST #7854
9000 EAST PIMA CENTER PARKWAY
SCOTTSDALE, ARIZONA 85258
PHONE (602) 685-7033

DEVELOPER

PEBBLECREEK PROP. LIMITED PARTNERSHIP
9532 E. RIGGS ROAD
SUN LAKES, ARIZONA 85248
PHONE (480) 895-9200

UNIT 59 PRELIMINARY PLAT / LAND USE DATA TABLE

SUBDIVISION UNIT	TOTAL LOTS	GROSS AREA (ACRES)	AREA OF PRIVATE STREETS (ACRES)	AREA OF OPEN SPACE, DRAINAGE, AND UTILITIES (ACRES)	AREA OF RESIDENTIAL DWELLING UNITS (ACRES)	GROSS DENSITY (DU/GROSS AREA)	MINIMUM LOT (SQ-FT)	MAXIMUM LOT (SQ-FT)	AVERAGE LOT (SQ-FT)
FIFTY-NINE	110	23.46	4.76	2.67	16.03	4.69	4,275	9,112	6,348

BASIS OF BEARINGS

THE BEARING OF N 89°40'22" W ALONG THE SOUTH LINE OF THE SOUTHWEST QUARTER OF SECTION 36, T2N, R2W, G & S.R.M.

PEBBLECREEK PHASE II
A PLANNED AREA DEVELOPMENT
PRELIMINARY PLAT - UNIT 59

B&R ENGINEERING INC.
9666 E. RIGGS RD. SUITE 118
SUN LAKES, ARIZONA 85248
PHONE: (480) 895-0799

DESIGN: CG
DRAWN: CV

CHECKED: JUL
APPROVED: JUL

41220
JEFFREY LEE
UHRICK
3-4-20
Data signed by
ARIZONA, U.S.A.

JOB NO.

970059

DATE

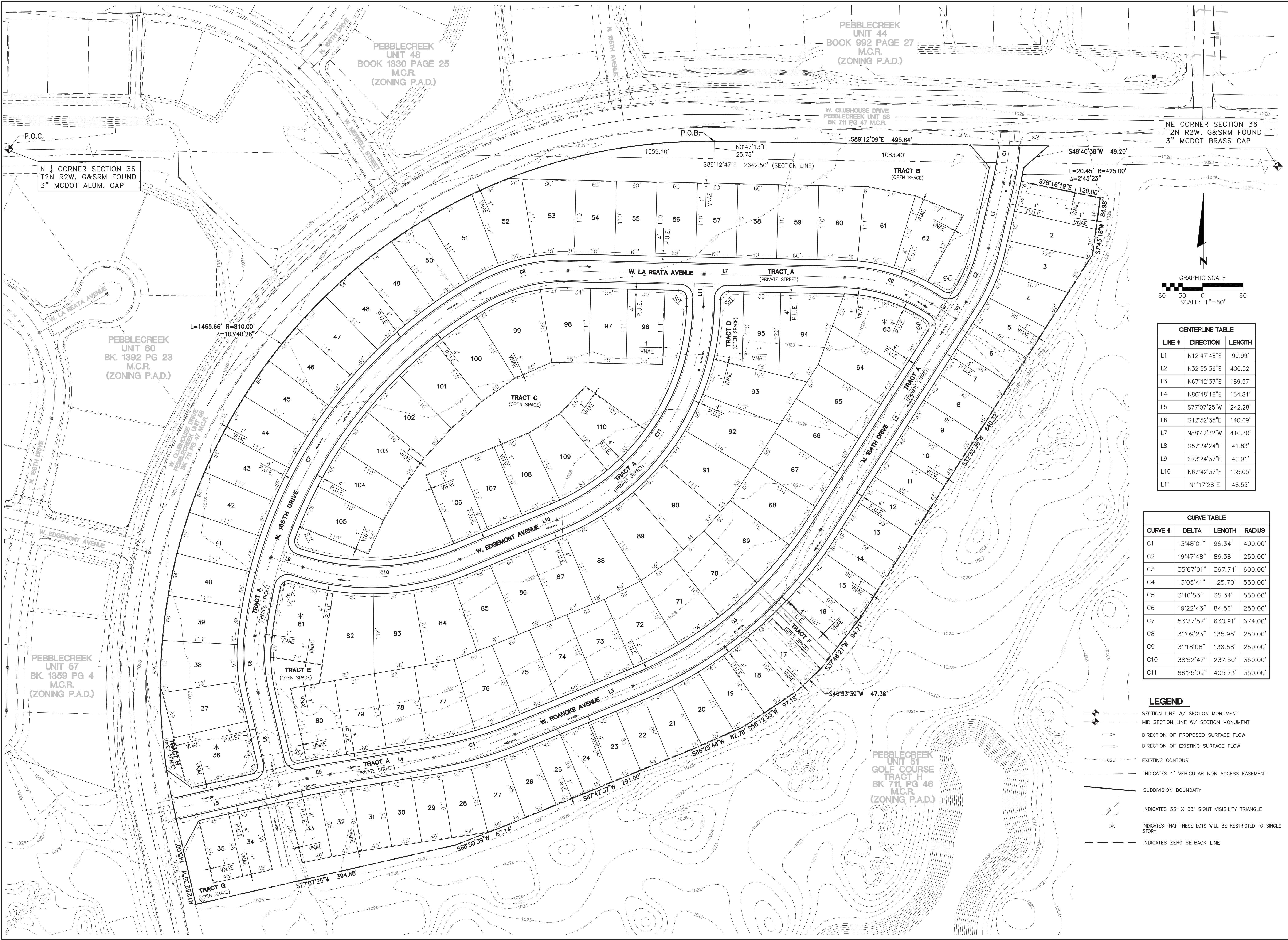
MARCH, 2020

SHEET

1 OF 2

DRAWER #

PEBBLECREEK PHASE II
A PLANNED AREA DEVELOPMENT
PRELIMINARY PLAT - UNIT 59
B&R ENGINEERING INC.
9666 E. RIGGS RD. SUITE 118
SUN LAKES, ARIZONA 85248
PHONE: (480) 895-0799
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41220
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3-4-20
Data signed by
ARIZONA, U.S.A.
JOB NO.
970059
DATE
MARCH, 2020
SHEET
1 OF 2
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CIVIL ENGINEERING © LAND DEVELOPMENT
© 2020 B & R ENGINEERING



NE CORNER SECTION 36
T2N R2W, G&SRM FOUND
3" MCDOT BRASS CAP

N 1/4 CORNER SECTION 36
T2N R2W, G&SRM FOUND
3" MCDOT ALUM. CAP

PEBBLECREEK
UNIT 60
BK. 1392 PG 23
M.C.R.
(ZONING P.A.D.)

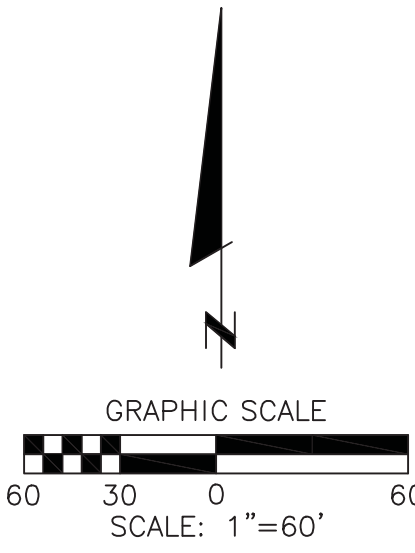
PEBBLECREEK
UNIT 57
BK. 1359 PG 4
M.C.R.
(ZONING P.A.D.)

PEBBLECREEK
UNIT 48
BOOK 1330 PAGE 25
M.C.R.
(ZONING P.A.D.)

PEBBLECREEK
UNIT 44
BOOK 992 PAGE 27
M.C.R.
(ZONING P.A.D.)

W. CLUBHOUSE DRIVE
PEBBLECREEK UNIT 58
BK 711 PG 47 M.C.R.

PEBBLECREEK
UNIT 51
GOLF COURSE
TRACT H
BK 711 PG 46
M.C.R.
(ZONING P.A.D.)



CENTERLINE TABLE		
LINE #	DIRECTION	LENGTH
L1	N12°47'48"E	99.99'
L2	N32°35'36"E	400.52'
L3	N67°42'37"E	189.57'
L4	N80°48'18"E	154.81'
L5	S77°07'25"W	242.28'
L6	S12°52'35"E	140.69'
L7	N88°42'32"W	410.30'
L8	S57°24'24"E	41.83'
L9	S73°24'37"E	49.91'
L10	N67°42'37"E	155.05'
L11	N1°17'28"E	48.55'

CURVE TABLE			
CURVE #	DELTA	LENGTH	RADIUS
C1	13°48'01"	96.34'	400.00'
C2	19°47'48"	86.38'	250.00'
C3	35°07'01"	367.74'	600.00'
C4	13°05'41"	125.70'	550.00'
C5	3°40'53"	35.34'	550.00'
C6	19°22'43"	84.56'	250.00'
C7	53°37'57"	630.91'	674.00'
C8	31°09'23"	135.95'	250.00'
C9	31°18'08"	136.58'	250.00'
C10	38°52'47"	237.50'	350.00'
C11	66°25'09"	405.73'	350.00'

- LEGEND**
- SECTION LINE W/ SECTION MONUMENT
 - MID SECTION LINE W/ SECTION MONUMENT
 - DIRECTION OF PROPOSED SURFACE FLOW
 - DIRECTION OF EXISTING SURFACE FLOW
 - EXISTING CONTOUR
 - INDICATES 1" VEHICULAR NON ACCESS EASEMENT
 - SUBDIVISION BOUNDARY
 - INDICATES 33' X 33' SIGHT VISIBILITY TRIANGLE
 - INDICATES THAT THESE LOTS WILL BE RESTRICTED TO 'SINGLE STORY'
 - INDICATES ZERO SETBACK LINE

PEBBLECREEK PHASE II
A PLANNED AREA DEVELOPMENT
PRELIMINARY PLAT - UNIT 59

BR ENGINEERING INC.
9666 E. RIGGS RD. SUITE 118
SUN LAKES, ARIZONA 85248
PHONE: (480) 895-0799

DESIGN
CG

CHECKED
JUL

DRAWN
CV

APPROVED
JUL

REGISTERED PROFESSIONAL ENGINEER
No. 41220
JEFFREY LEE UHRICK
ARIZONA, U.S.A.

3-4-20

JOB NO.
970059

DATE
MARCH, 2020

SHEET
2 OF 2

PEBBLECREEK UNIT 59
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